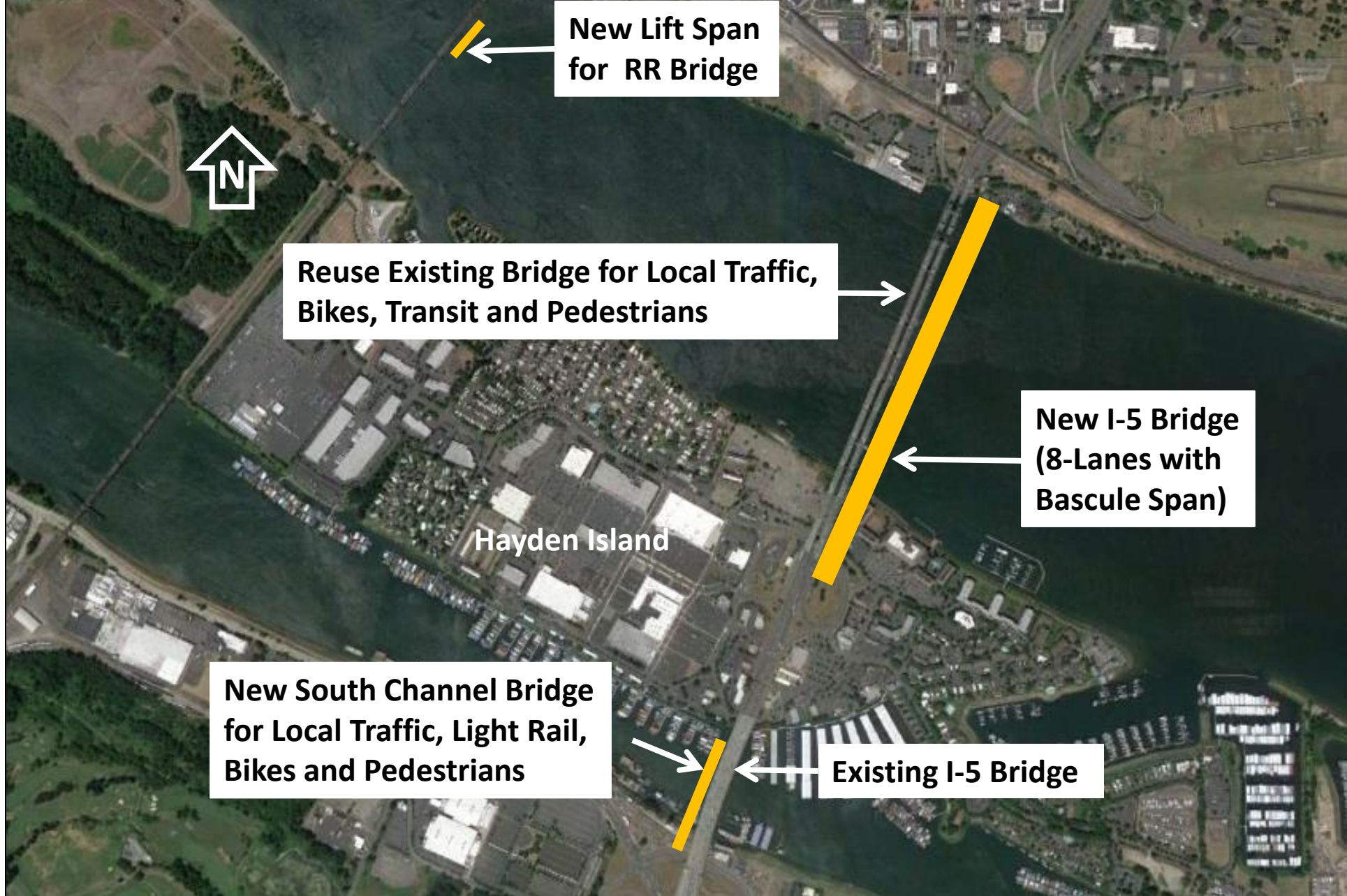


Common Sense Alternative II (CSA) Includes Reusing the Existing Bridge for Local Traffic, Buses & Bikes

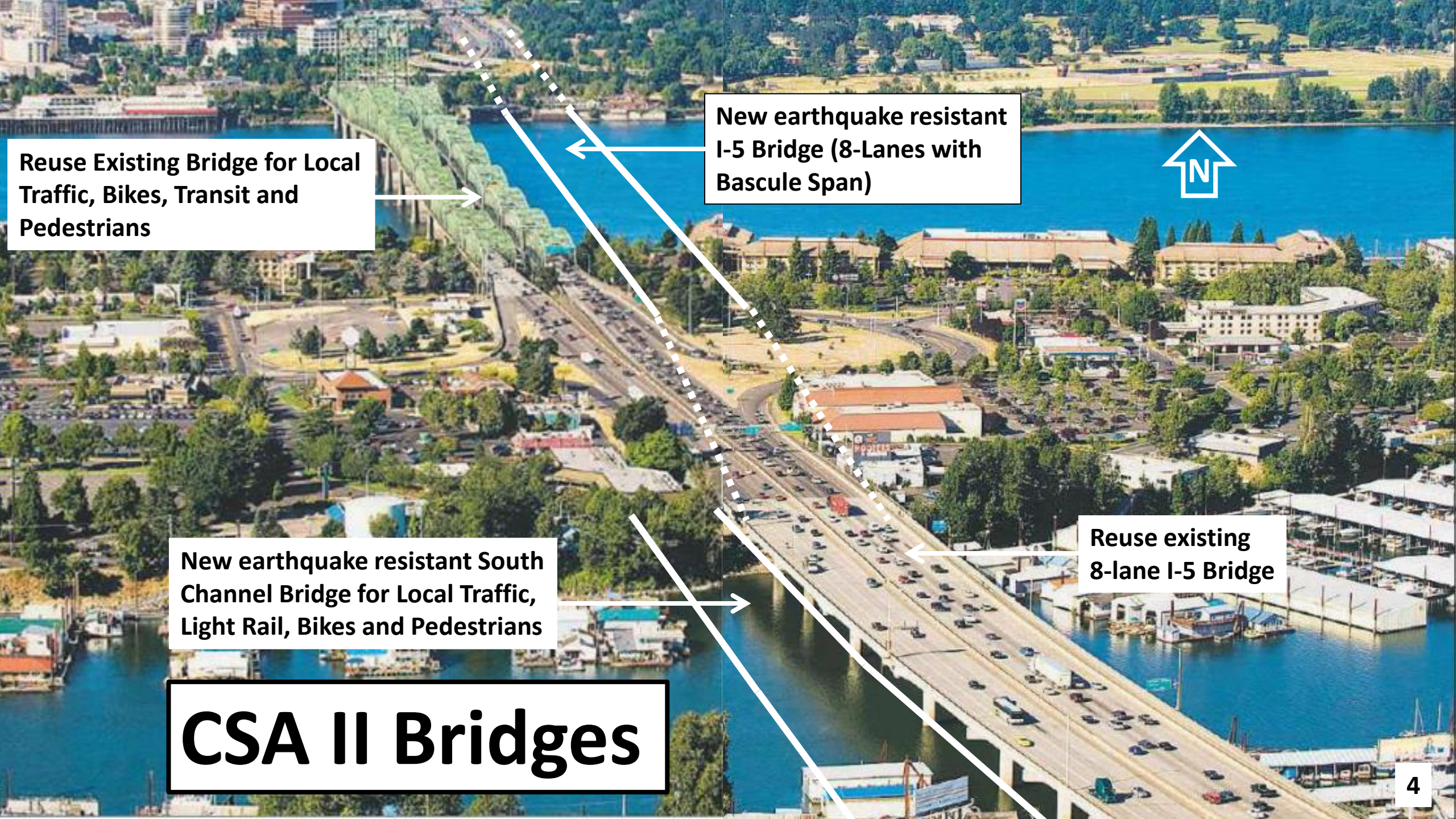




Common Sense Alternative II

Purpose and Needs

1. Growing travel demand and congestion
2. Impaired freight movement
3. Limited public transportation operation, connectivity, reliability **and equity**
4. Safety and vulnerability to incidents
5. Substandard bicycle and pedestrian facilities
6. Seismic vulnerability
7. **Addresses GHG emissions and climate change**



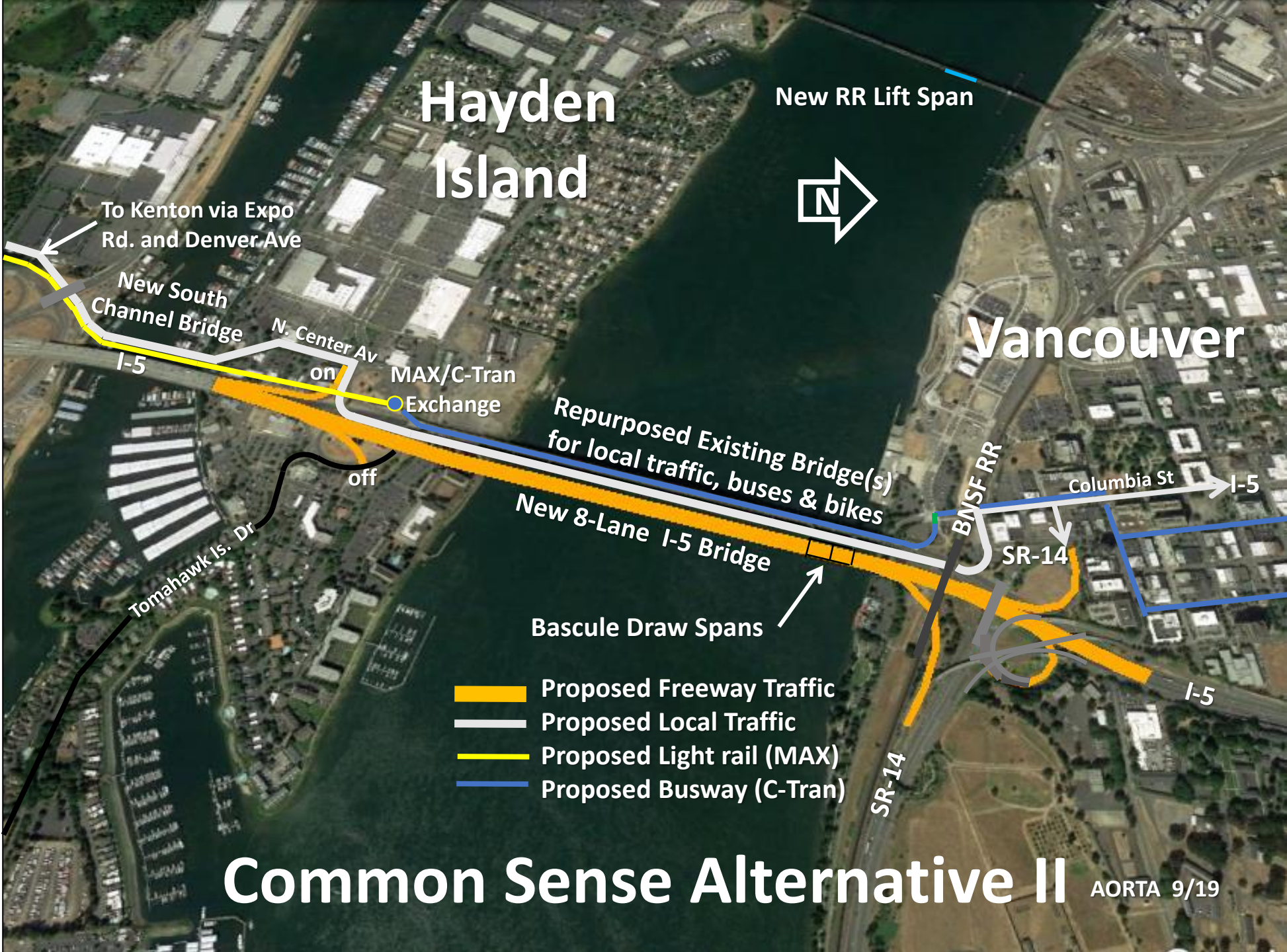
Reuse Existing Bridge for Local Traffic, Bikes, Transit and Pedestrians

New earthquake resistant I-5 Bridge (8-Lanes with Bascule Span)

New earthquake resistant South Channel Bridge for Local Traffic, Light Rail, Bikes and Pedestrians

Reuse existing 8-lane I-5 Bridge

CSA II Bridges



Hayden Island

New RR Lift Span



Vancouver

To Kenton via Expo Rd. and Denver Ave

New South Channel Bridge

N. Center Av

MAX/C-Tran Exchange

Repurposed Existing Bridge(s) for local traffic, buses & bikes

New 8-Lane I-5 Bridge

Bascule Draw Spans

Tomahawk Is. Dr

BNSF RR

Columbia St

SR-14

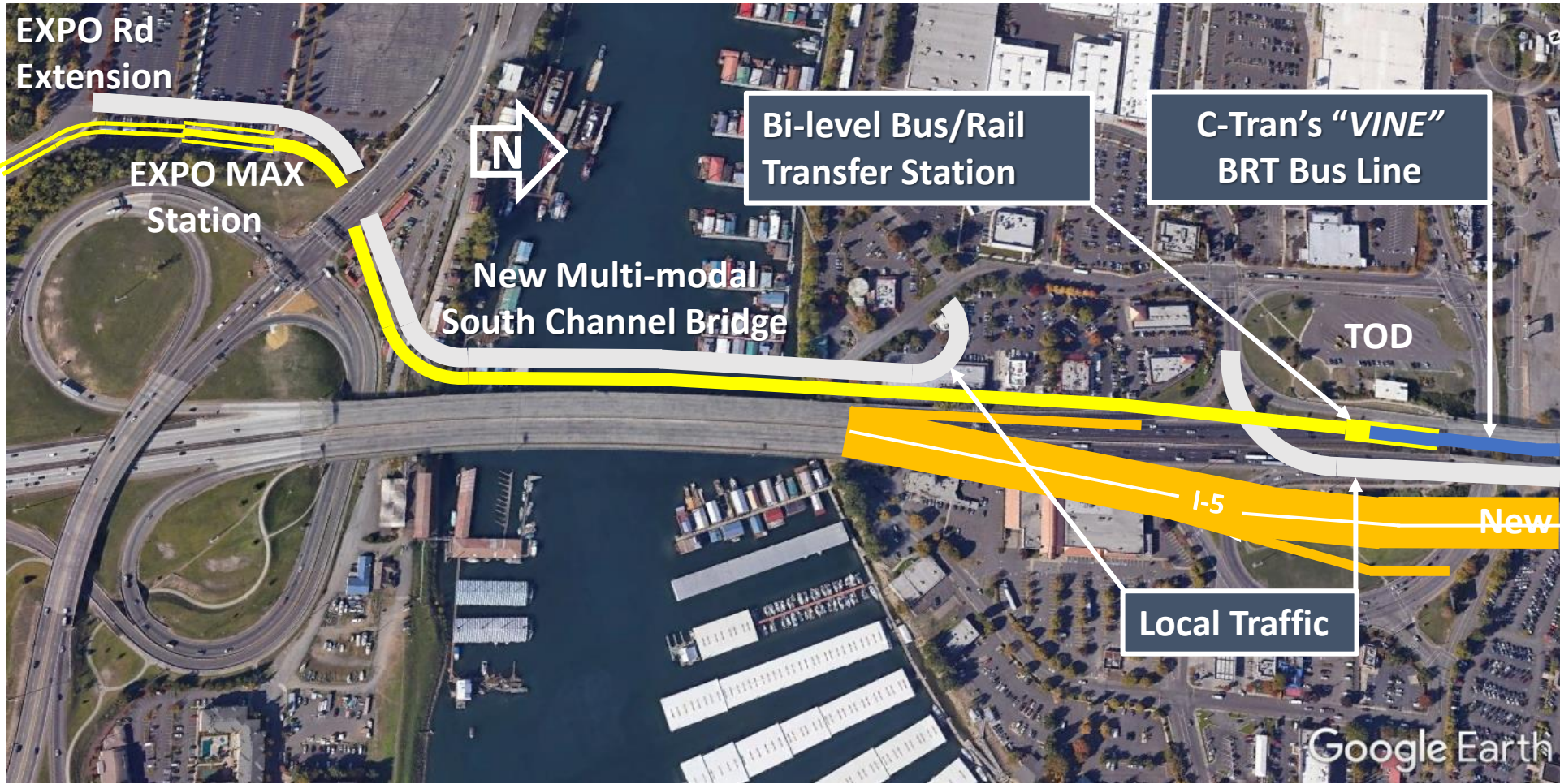
I-5

SR-14

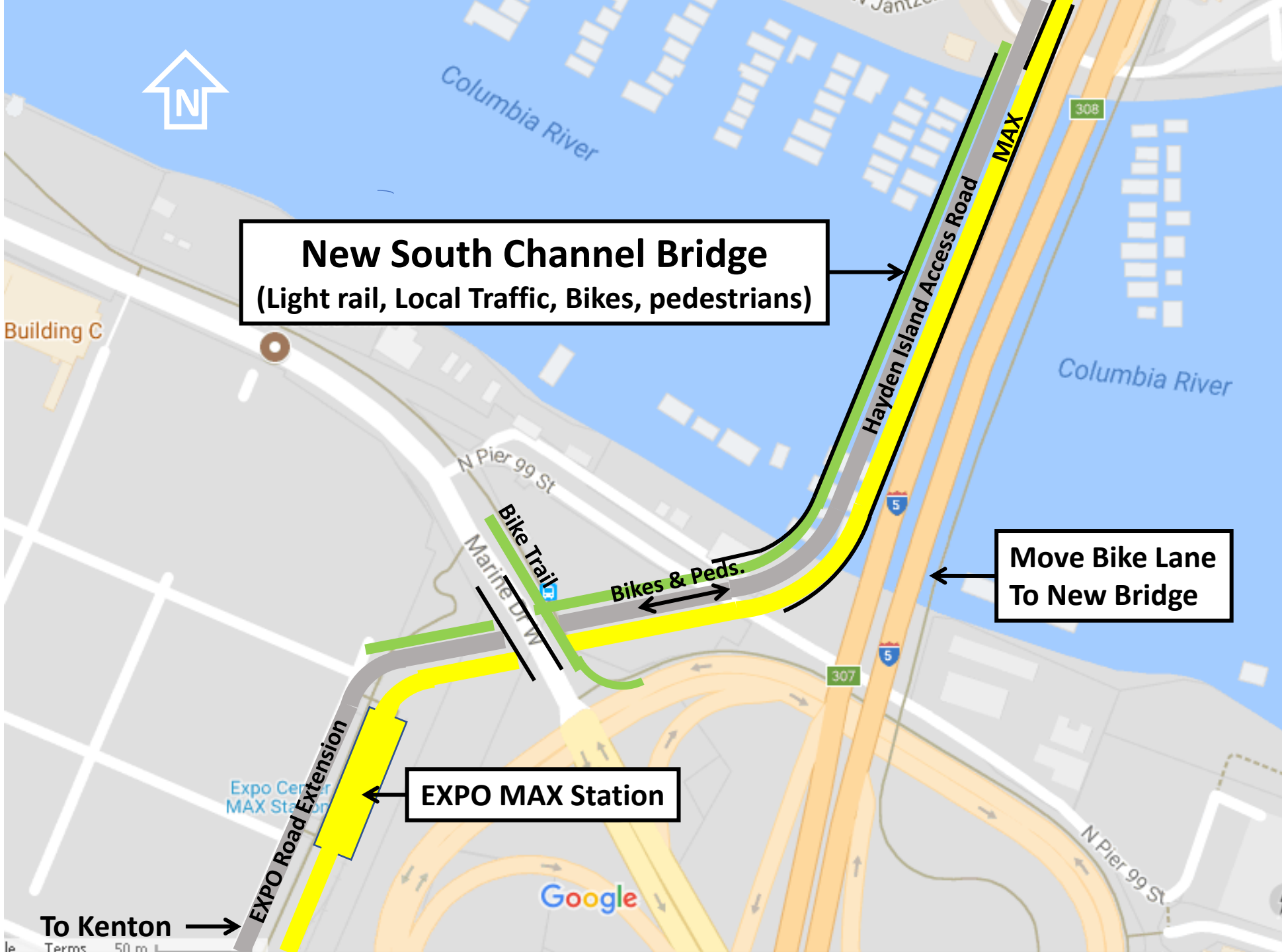
- Proposed Freeway Traffic
- Proposed Local Traffic
- Proposed Light rail (MAX)
- Proposed Busway (C-Tran)

Common Sense Alternative I

AORTA 9/19



Existing I-5/Marine Drive Interchange with no I-5/Hayden Island Interchange

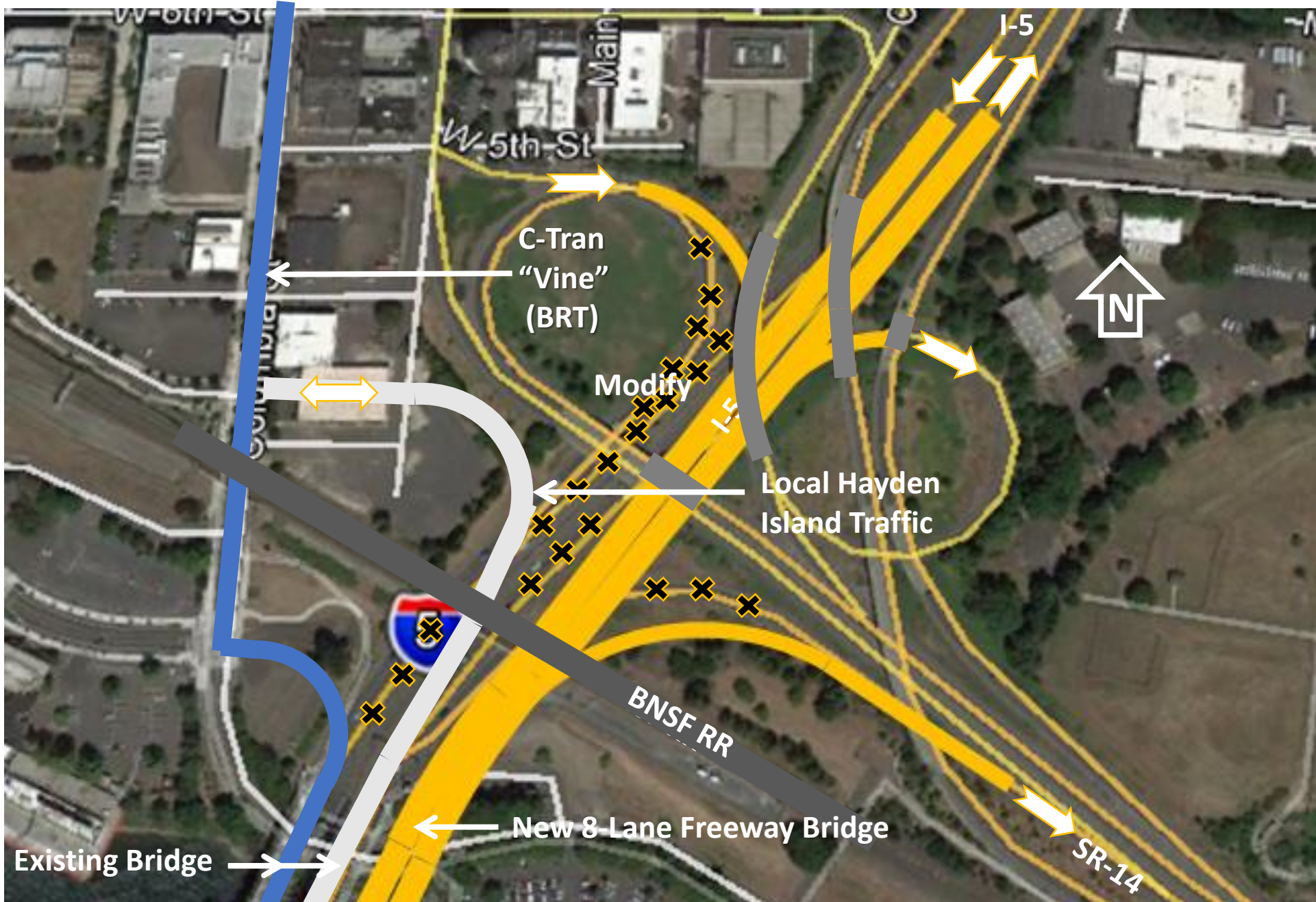


New South Channel Bridge
(Light rail, Local Traffic, Bikes, pedestrians)

Move Bike Lane To New Bridge

EXPO MAX Station

To Kenton →



At-Grade Vancouver Interchange

Existing Hayden Island

Columbia River



Hayden Island

Existing Bridge(s)

New Bridge



N. Hayden Island Dr

N. Center Av

Transit Transfer Station

Bus Rapid Transit & Bikes
Local Traffic (2-lanes)

Columbia River

Interstate-5 (8-lanes)

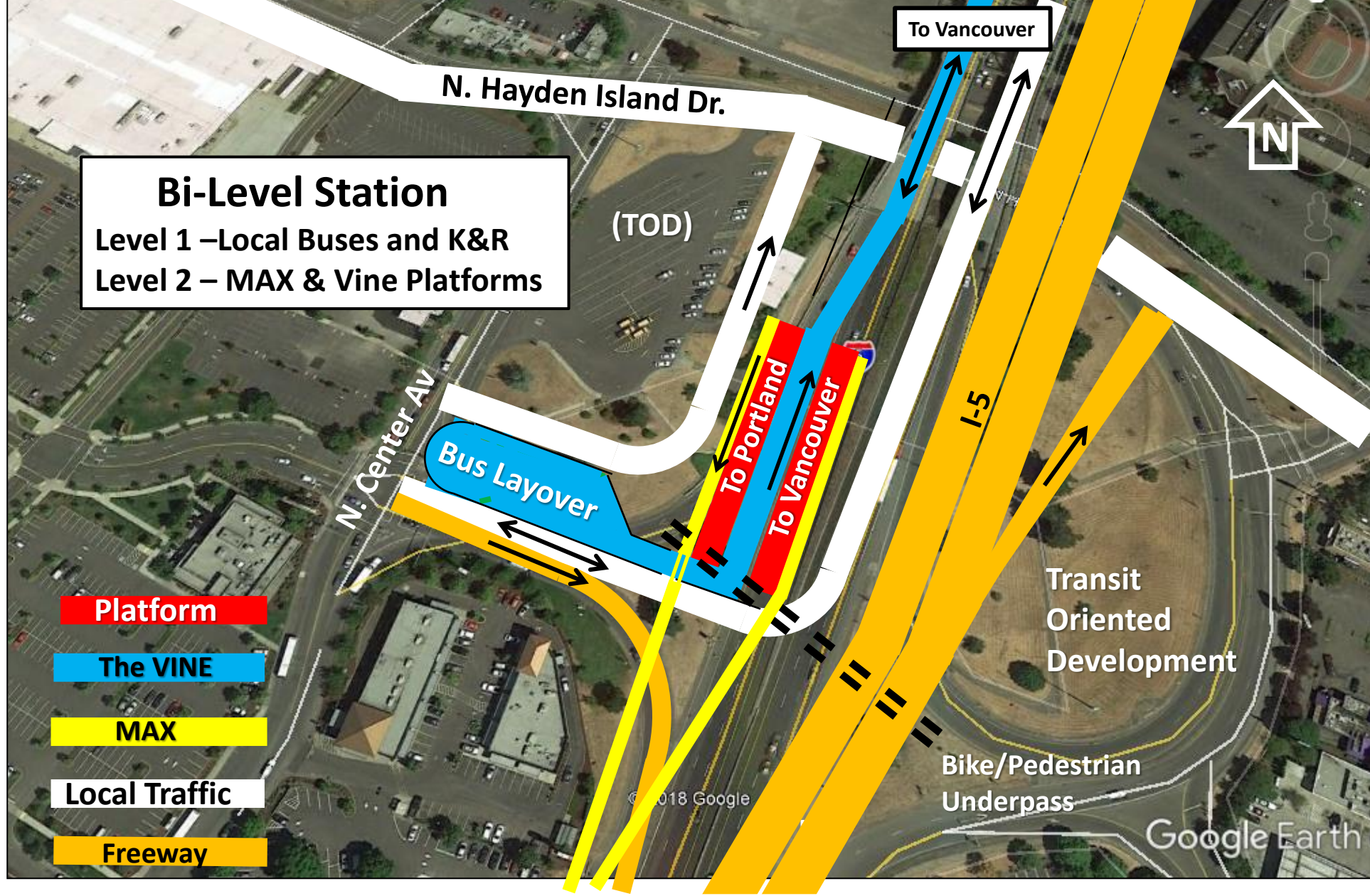
Bike/ped Underpass

N. Jantzen Dr

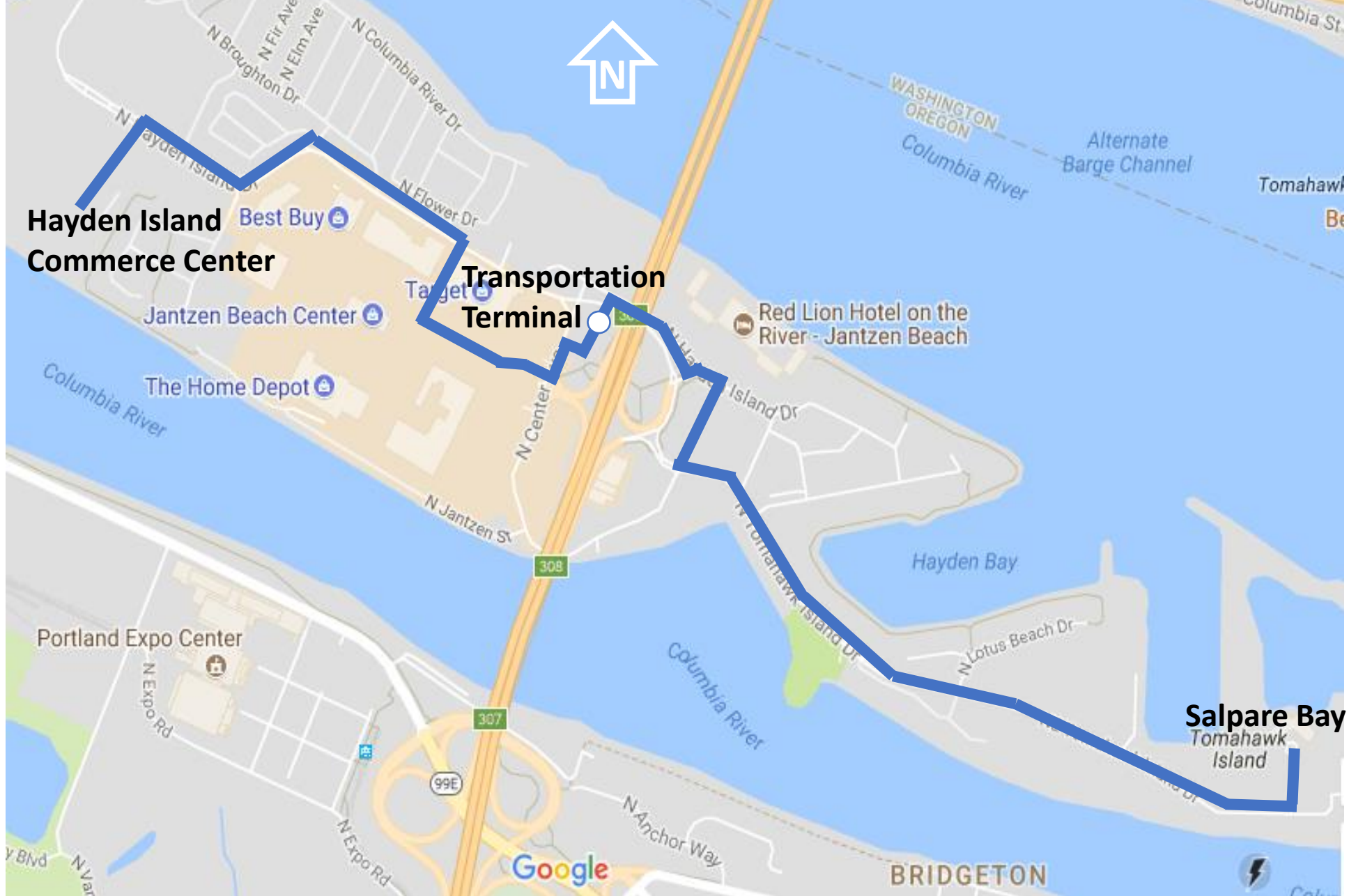
MAX LRT & Bikes (elevated)

Transit Oriented Development Site

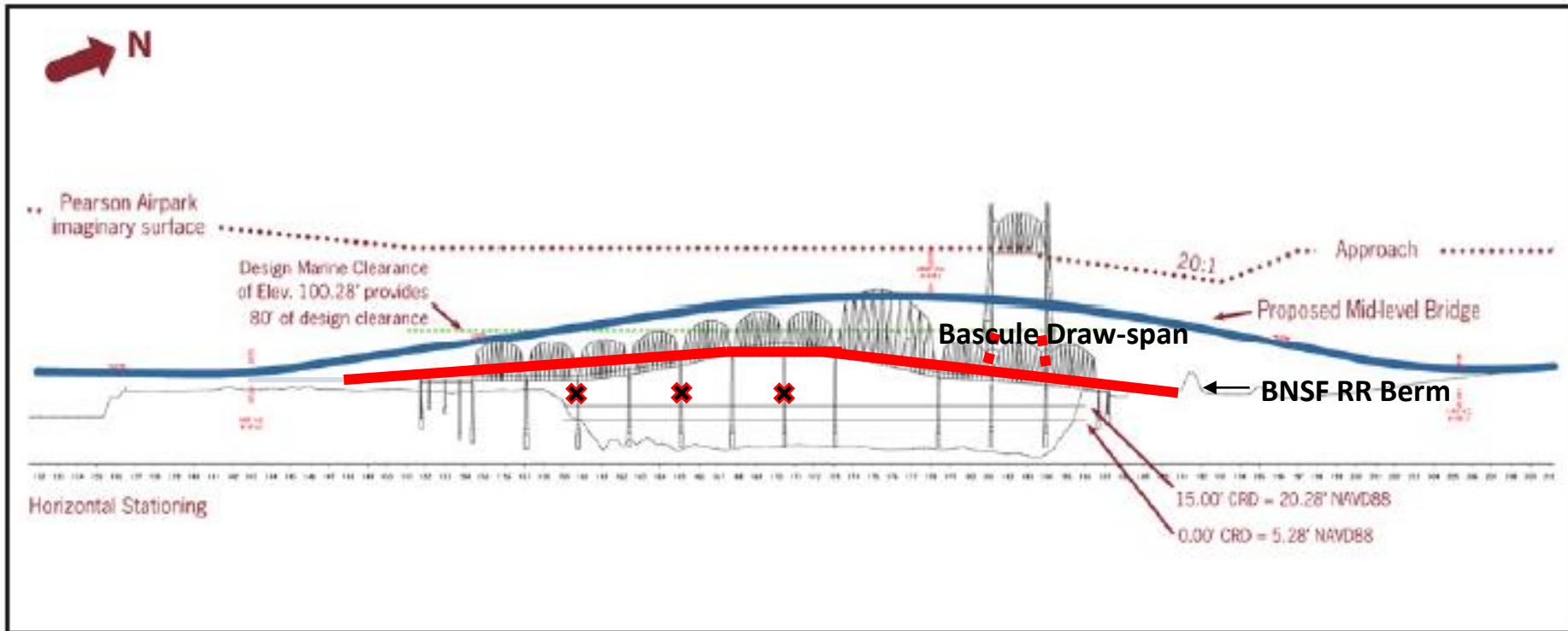




Hayden Island Bus to Rail Transfer Station

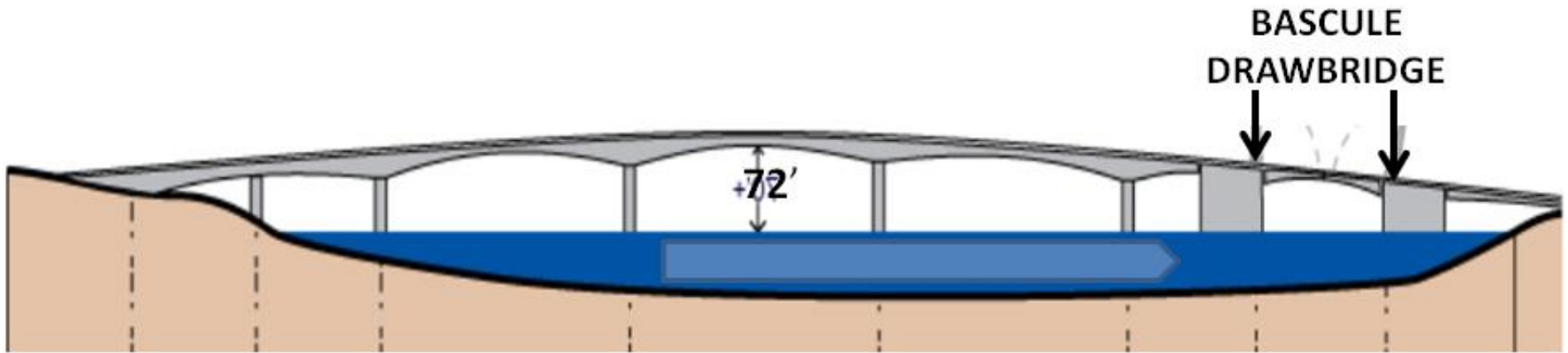


Hayden Island Shuttle Bus

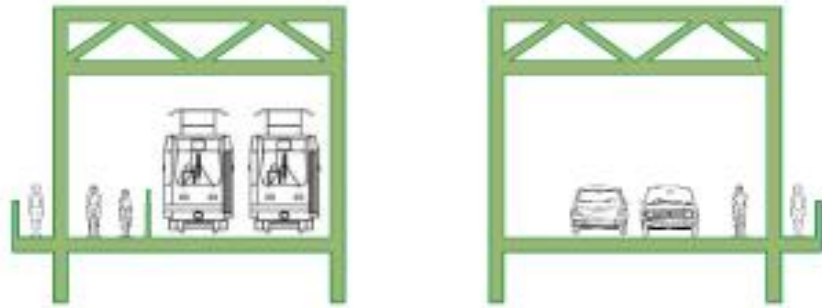


- ODOT's Preferred Alternative (95')**
- Common Sense Alternative II (72')**

Profiles



CSA II Bridge Looking West



Transit (Buses/LRT)
Cycle-Track & Ped.

Local traffic
& Pedestrians

Existing I-5 Lift Span Bridges



Freeway – 3 through lanes
+ add/drop lane each way

New I-5 Bascule Bridge

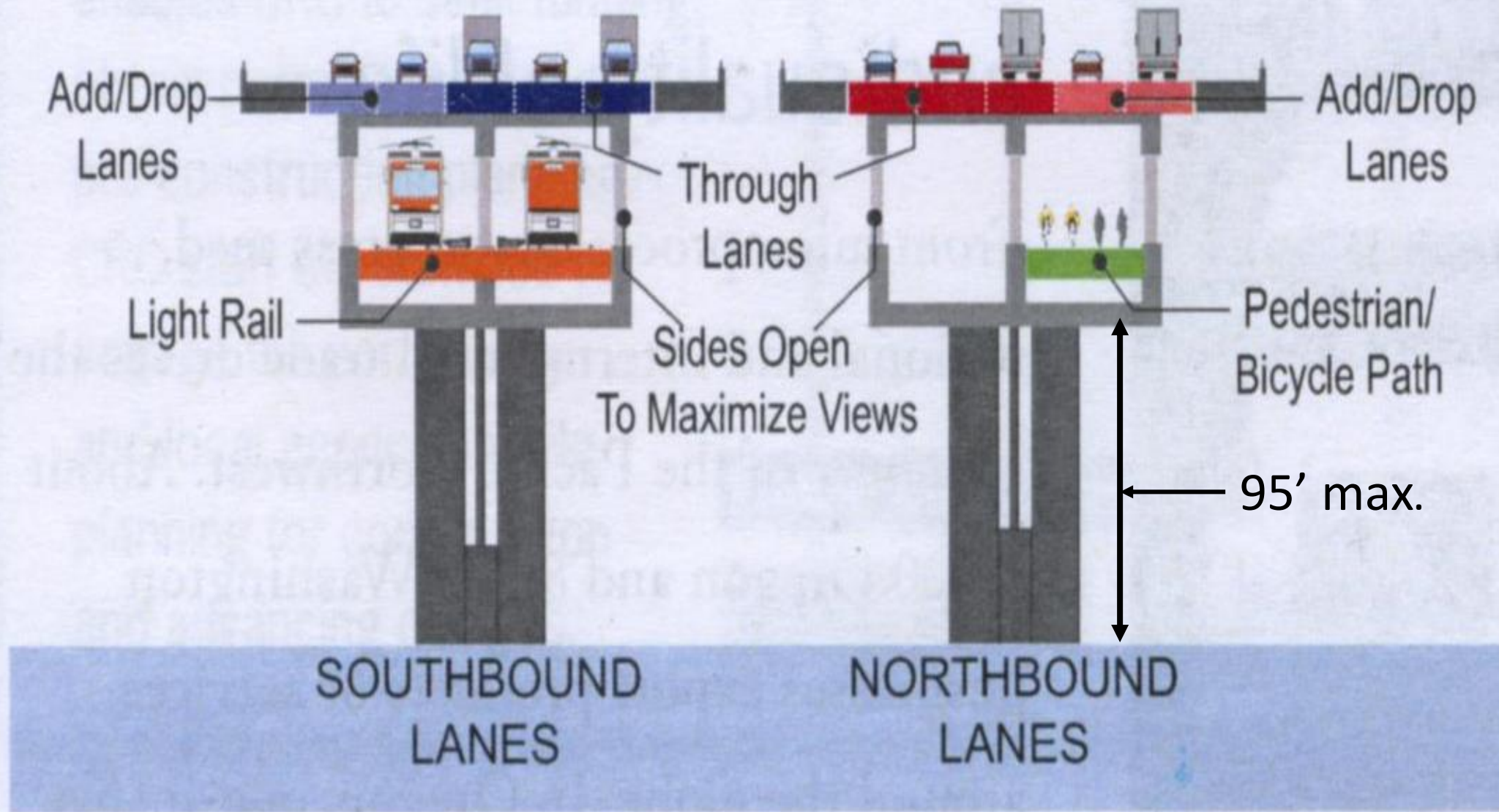
72' River
Clearance

(Cross-Section looking North toward Vancouver)

Common Sense Alternative II



CSA PROPOSAL - Bascule draw span similar to this new Woodrow Wilson I-95 Bridge near Washington DC



Design calls for two structures with a total of 10 lanes and full safety shoulders.

(ODOT's Proposal)

550 Annual
I-5 Lifts for Vessels
Under 60' Height

72' Clearance
Under I-5



BRIDGE LIFT PATH ▶
High river flow or
vessels that require
more than 68 ft.

Lift
Span

Primary
Channel

Barge Channel

Alternate
Barge Channel

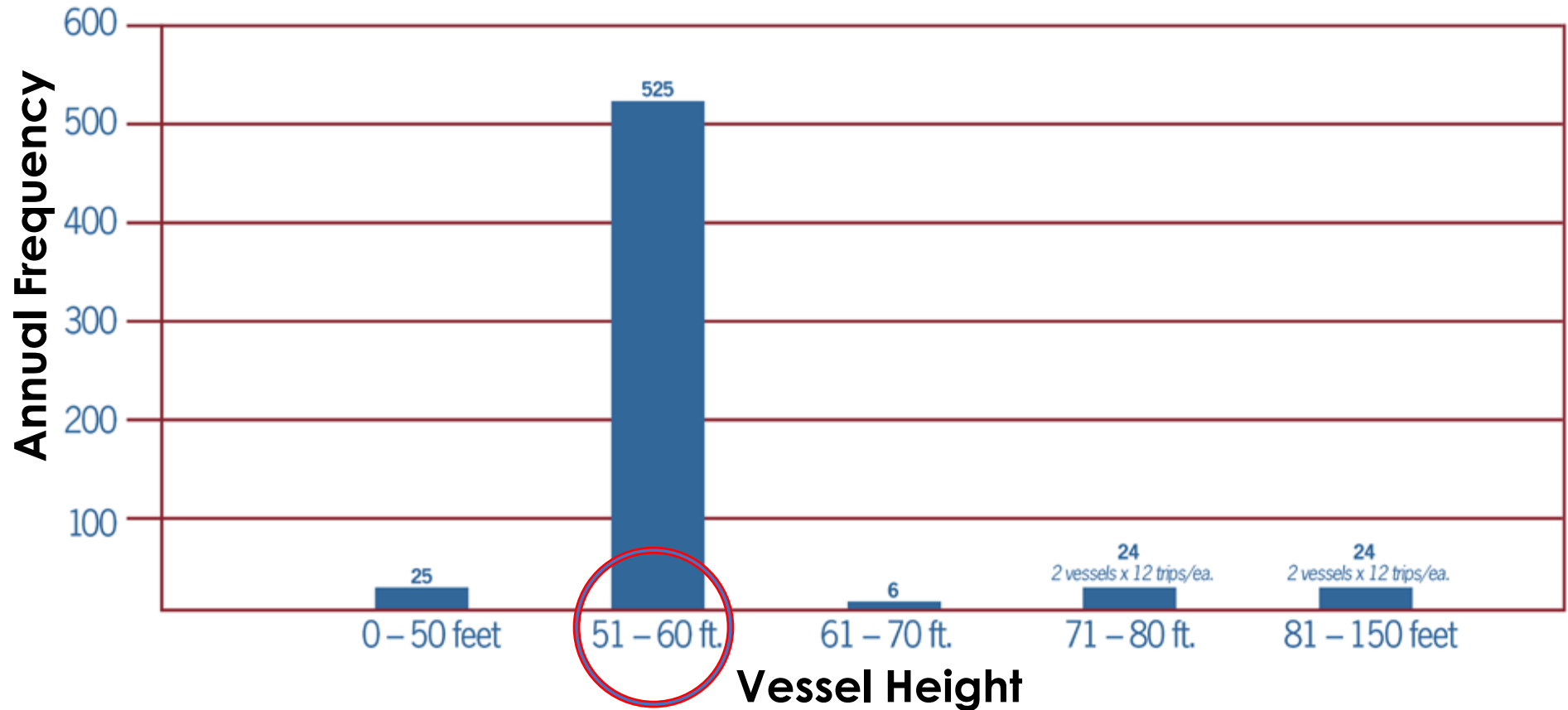
Rotating
Span

Existing Barge Traffic



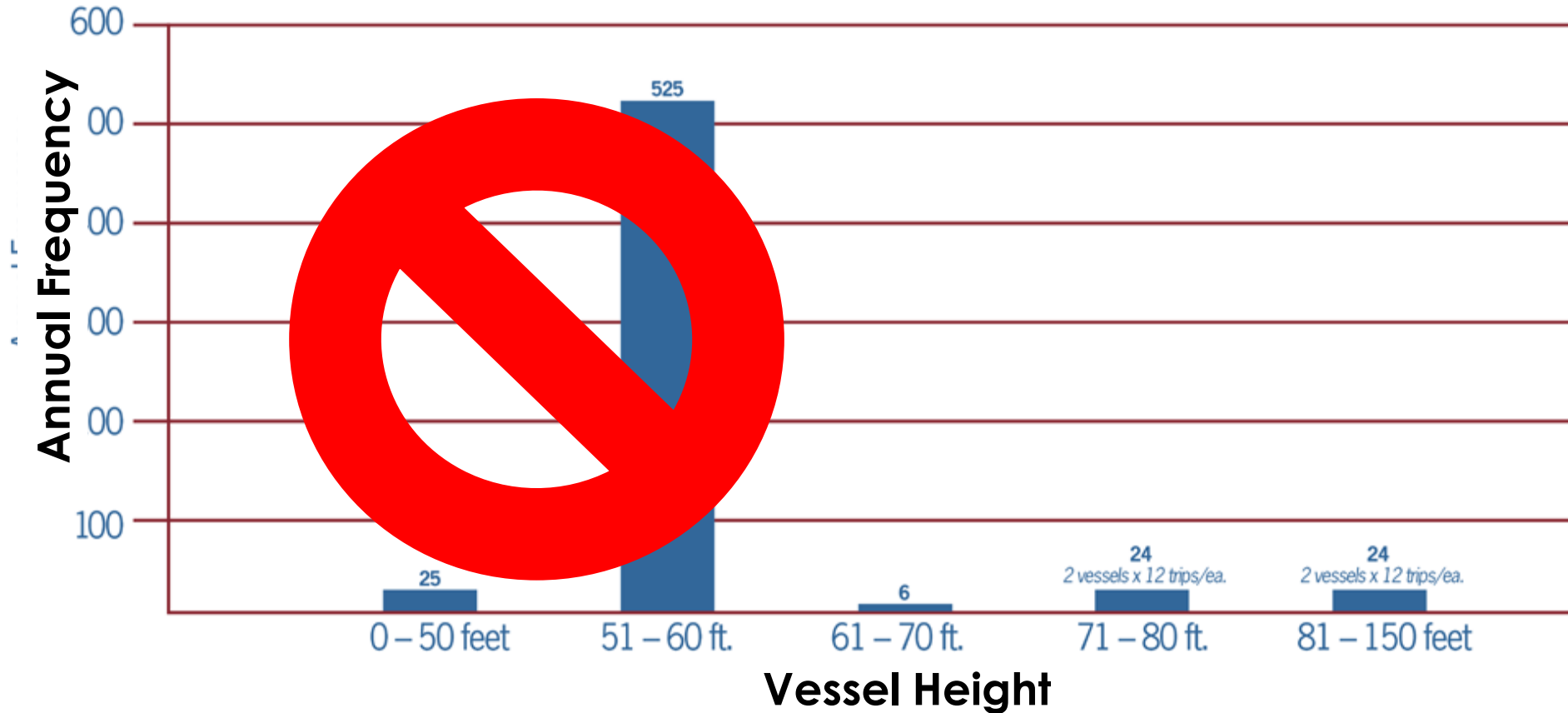
BNSF Railroad New Lift Span

604 Total I-5 Lifts

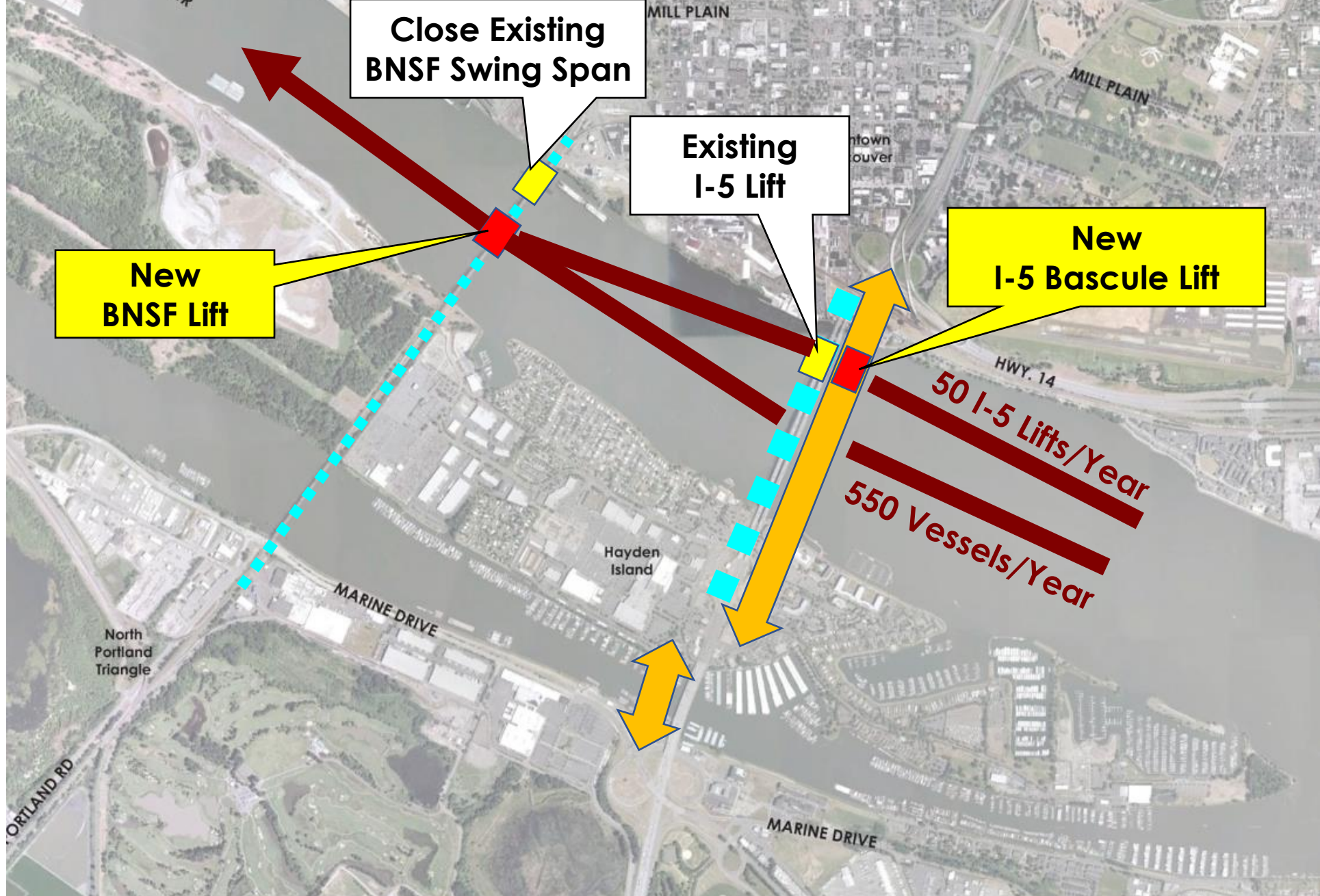


I-5 Bridge Lift Frequency (2004 Averages)

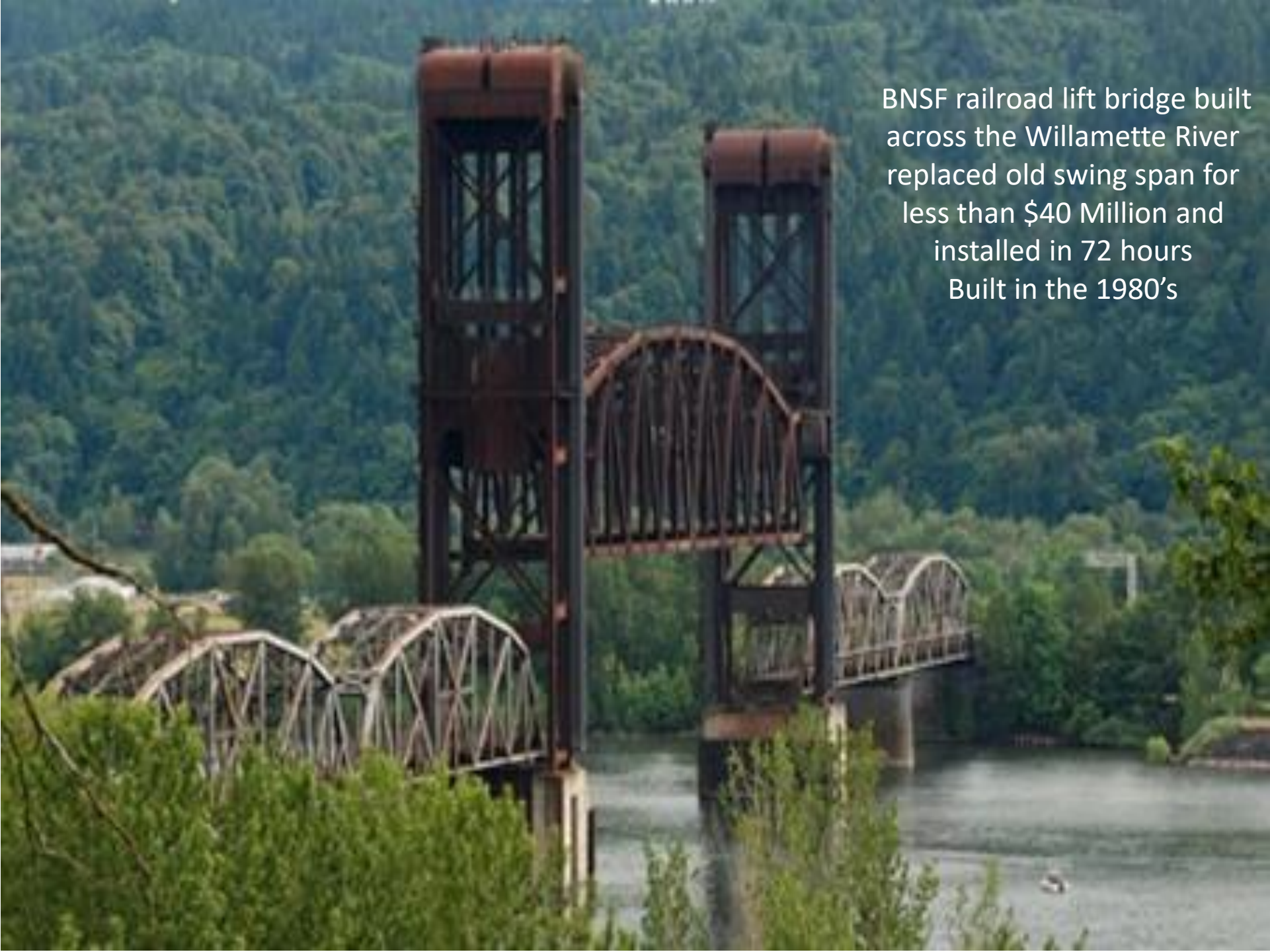
550 Lifts Eliminated



I-5 Bridge Lift Frequency (2004 Averages)



Barge Traffic With New Bridge



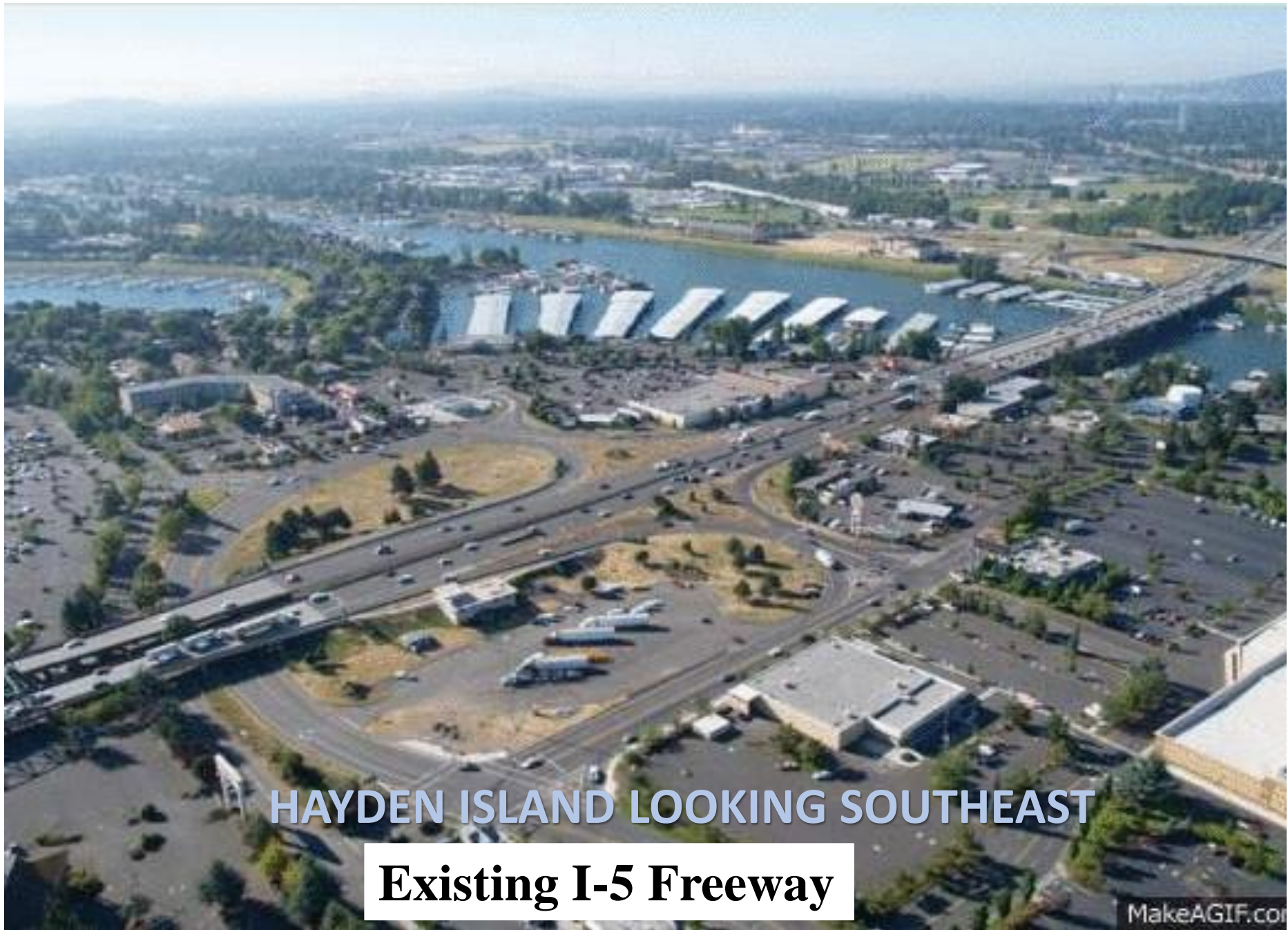
BNSF railroad lift bridge built across the Willamette River replaced old swing span for less than \$40 Million and installed in 72 hours
Built in the 1980's

Common Sense Alternative II

The Common Sense Alternative II is a workable crossing of the Columbia between Portland and Vancouver. It would **eliminate the need for a full interchange on Hayden island.**

- **Install a lift span in the railroad bridge** downriver from the existing Interstate Bridges. This would allow all commodity barge traffic to navigate under the high spans of the existing Interstate Bridges and reduce the number of lifts by 90 percent.
- **Construct a new eight-lane freeway bridge with a bascule opening** that aligns with the lift span of the existing bridges. This bridge would accommodate river traffic of any height and align exceptionally well with existing Interstate-5 approaches. I-5 can continue to cross beneath the BNSF railroad. Its low profile solves many of the engineering challenges of the CRC. This opening span is not unprecedented on a major Interstate Highway. (I-95 Bridge recently built near Washington, DC.)
- **Repurpose the existing Interstate Bridge** for local traffic, public transit, bikes and pedestrians. Seismic retrofitting would be an option, not a requirement.
- **Build a new bridge over the South Channel** for local traffic, light rail, bikes and pedestrians that allows non-freeway vehicle access between North Portland and Hayden Island.

**The Next Slides Compare the
Common Sense Alternative II
To the CRC Preferred Alternative**



HAYDEN ISLAND LOOKING SOUTHEAST

Existing I-5 Freeway



ODOT's Columbia River Crossing Concept

Columbia River Crossing conceptual drawing, looking south, showing the new bridge with light rail access along the west (lower right in drawing) side.

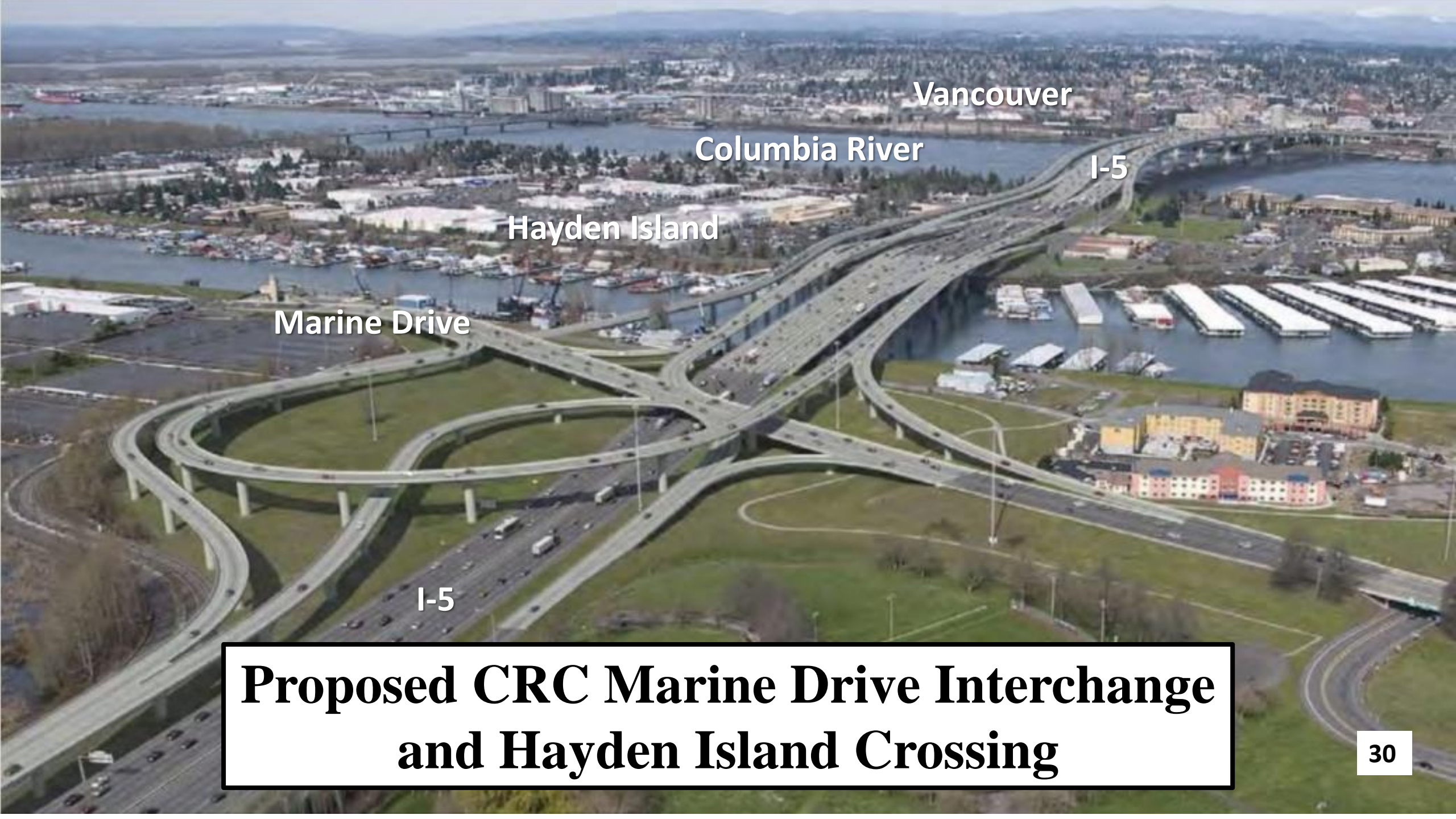


HAYDEN ISLAND LOOKING SOUTHEAST

Common Sense Alternative II

Existing Marine Drive Interchange





Vancouver

Columbia River

I-5

Hayden Island

Marine Drive

I-5

**Proposed CRC Marine Drive Interchange
and Hayden Island Crossing**

Light Rail and Local Traffic Bridge With Existing Marine Drive Interchange





Looking South from Vancouver



ODOT's Preferred Alternative



Common Sense Alternative II